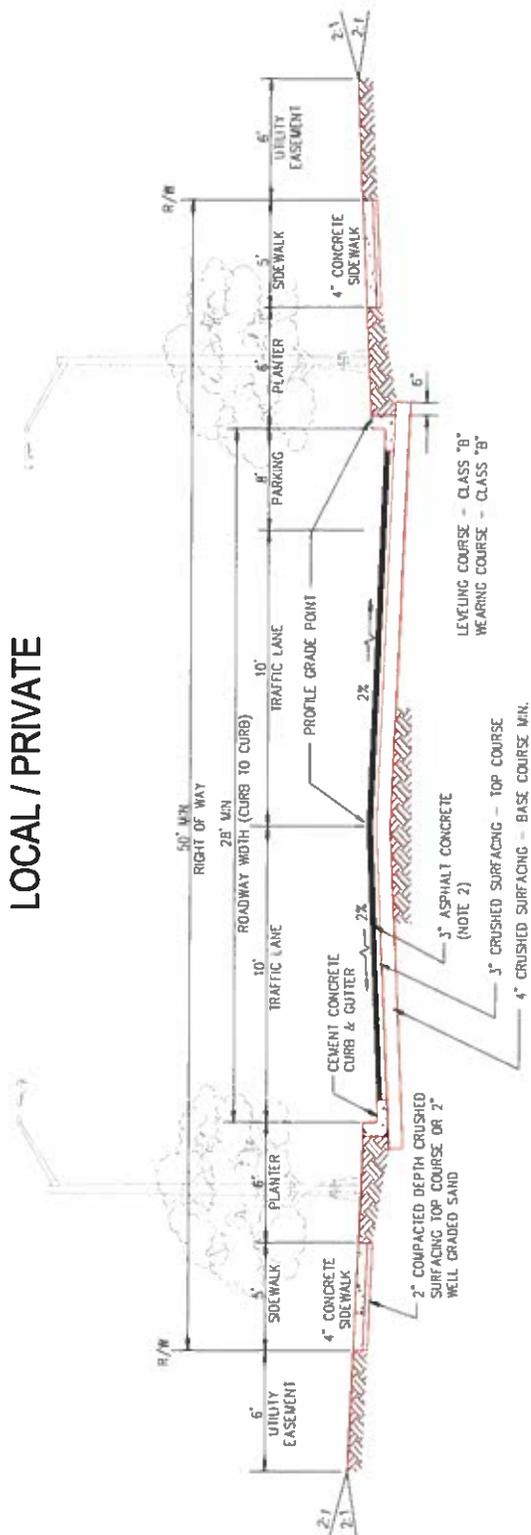


SECTION SIX TABLE OF CONTENTS

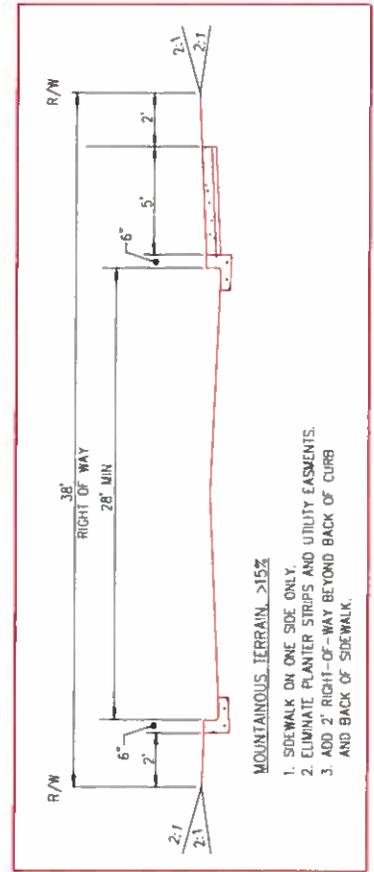
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LOCAL/PRIVATE STREET DESIGN

LOCAL / PRIVATE



- NOTES:
1. ALL THICKNESSES ARE COMPACTED DEPTHS.
 2. CONTRACTOR WILL BE REQUIRED TO PAVE NEW ROADWAY IN TWO (2) EQUAL LIFTS, UNLESS PERMISSION IS GRANTED BY CITY ENGINEER TO PAVE IN ONE LIFT.
 3. PRIVATE ROADS SHALL BE APPROVED BY CITY ENGINEER (WIDTHS MAY BE MODIFIED).

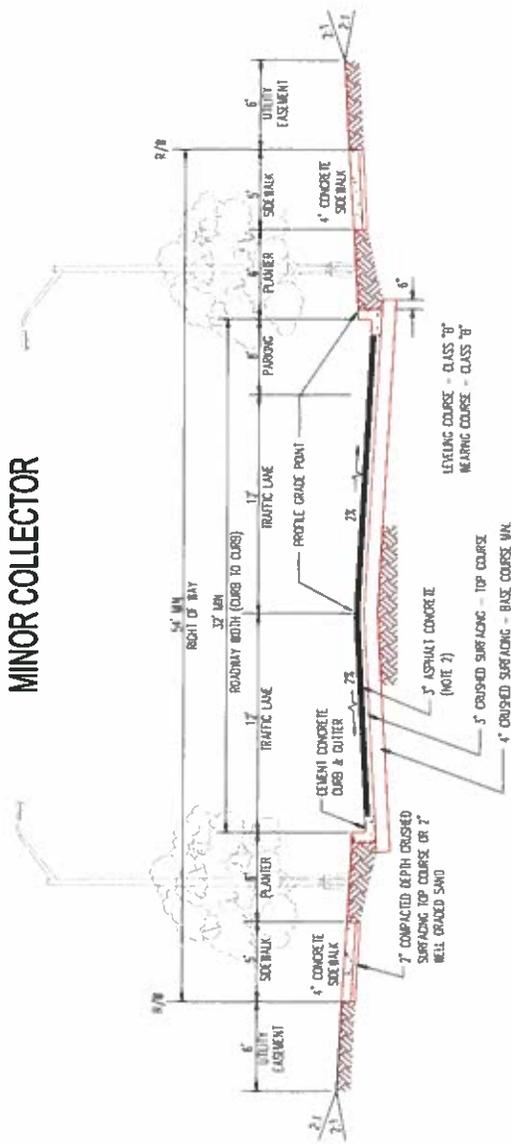


- MOUNTAINOUS TERRAIN >15%
1. SIDEWALK ON ONE SIDE ONLY.
 2. ELIMINATE PLANTER STRIPS AND UTILITY EASEMENTS.
 3. ADD 2' RIGHT-OF-WAY BEYOND BACK OF CURB AND BACK OF SIDEWALK.

10-073

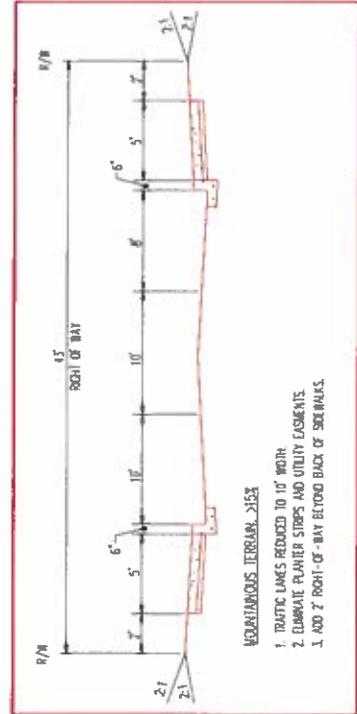
MINOR COLLECTOR STREET DESIGN

MINOR COLLECTOR



NOTES:

1. ALL FINISHES ARE COMPACTED REPAIRS
2. CONTRACTOR WILL BE REQUIRED TO PAVE NEW ROADWAY IN TWO (2) EQUAL LOTS, UNLESS PERMISSION IS GRANTED BY CITY ENGINEER TO PAVE IN ONE LOT.



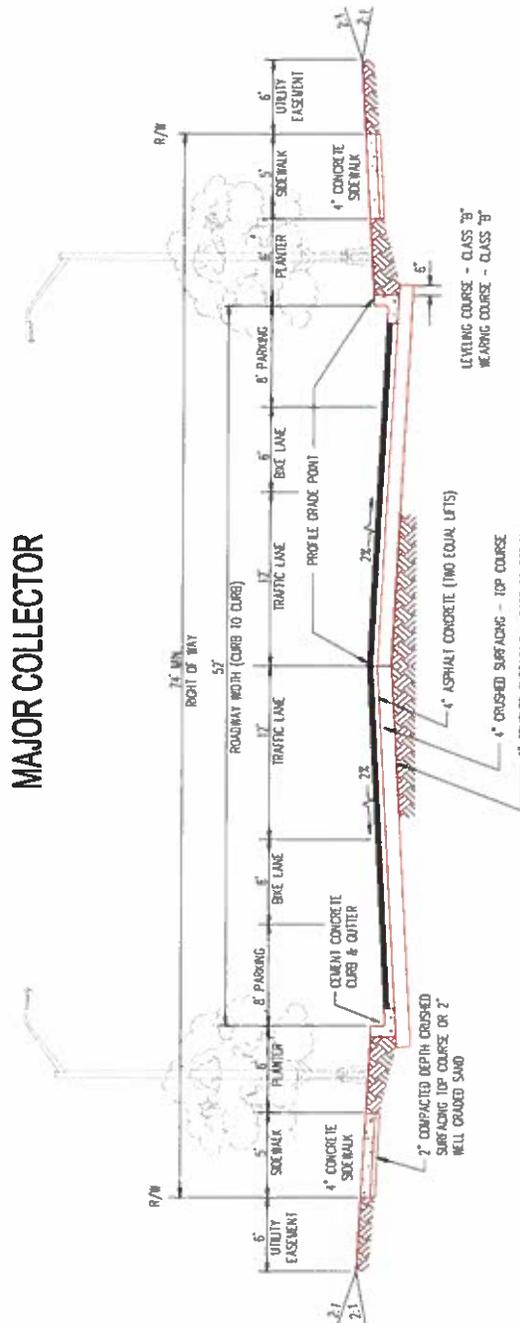
MOUNTAINOUS TERRAIN - 2:1S

1. TRAFFIC LANES REDUCED TO 10' WIDTH
2. ELIMINATE PLANTER STRIPS AND UTILITY EASEMENTS.
3. ADD 2' RIGHT-OF-WAY BEYOND BACK OF SIDEWALKS.

ID-071

MAJOR COLLECTOR STREET DESIGN

MAJOR COLLECTOR

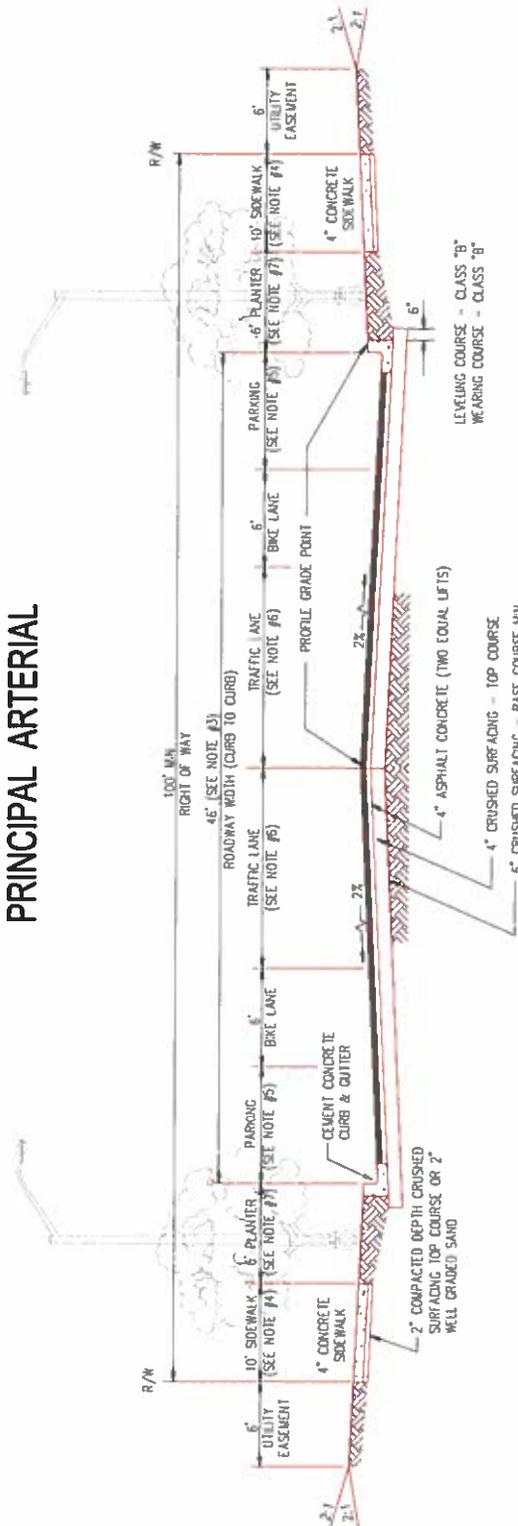


- NOTES:
1. ALL THICKNESSES ARE COMPACTED DEPTHS.
 2. CONTRACTOR WILL BE REQUIRED TO PAVE NEW ROADWAY BY TWO (2) EQUAL LIFTS, UNLESS PERMISSION IS GRANTED BY CITY ENGINEER TO PAVE IN ONE LIFT.

ID-072

ARTERIAL STREET DESIGN

PRINCIPAL ARTERIAL



NOTES:

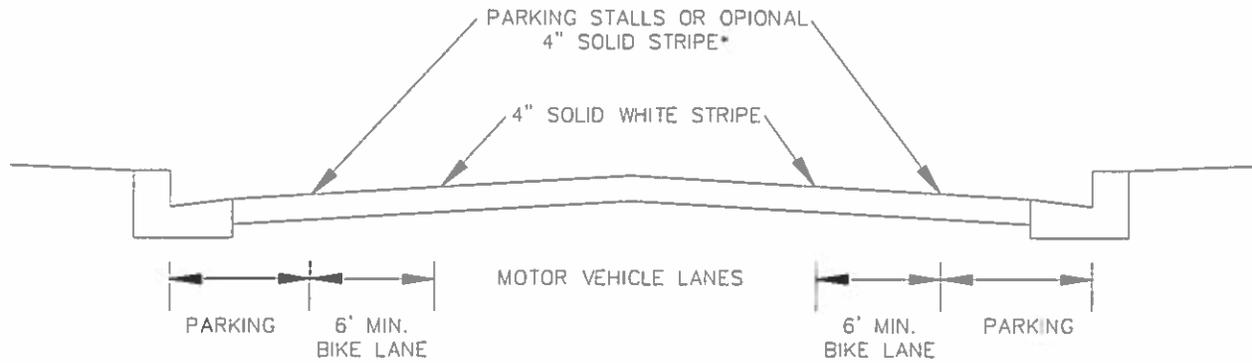
1. ALL THICKNESSES ARE COMPACTED DEPTHS.
2. CONTRACTOR WILL BE REQUIRED TO PAVE NEW ROADWAY IN TWO (2) LIFTS.
3. TWO 11-FOOT LANES, ONE 12-FOOT CENTER LANE, AND TWO 6-FOOT BIKE LANES. ACTUAL WIDTH WILL BE DETERMINED BY NUMBER OF TRAVEL LANES BASED ON THE PROJECTED VOLUME OF TRAFFIC.
4. THE MINIMUM SIDEWALK WIDTH WILL BE SIX FEET FOR ALL OTHER AREAS NOT DESIGNATED WITHIN CITY PLANNING DOCUMENTS.
5. PARKING LANE REQUIRED IN DESIGNATED AREAS PER APPROVED PLANNING DOCUMENTS.
6. ACTUAL WIDTH WILL BE DETERMINED BASED ON TWO-FIVE TRAVEL LANES DEPENDING ON PROJECTED VOLUME OF TRAFFIC USING THE FACILITY (REFER TO TRANSPORTATION PLAN.)
7. PLANTER STRIPS WILL BE REQUIRED FOR ALL AREAS WHERE THE SIDEWALK WIDTH IS BETWEEN 5-8 FEET; OTHERWISE, STREET TREES MAY BE INCLUDED WITHIN THE 10-FOOT SIDEWALK WIDTH. THE CITY ENGINEER WILL MAKE THE FINAL DETERMINATION.

0-025

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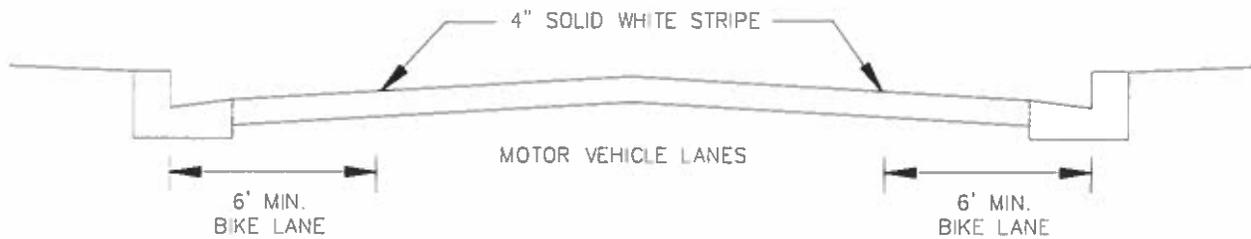
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BIKE LANES



* THE OPTIONAL SOLID WHITE STRIPE MAY BE ADVSABLE WHERE STALLS ARE UNNECESSARY (BECAUSE PARKING IS LIGHT) BUT THERE IS CONCERN THAT MOTORISTS MAY MISCONSTURE THE BIKE LANE TO BE A TRAFFIC LANE.

DESIGN-A STRIPED PARKING

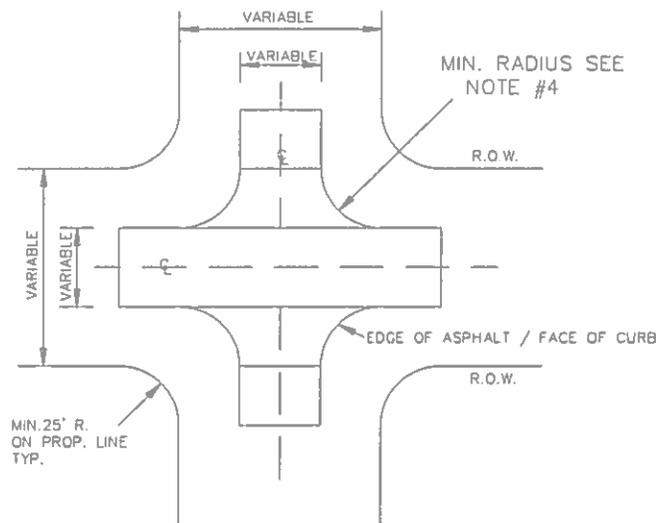
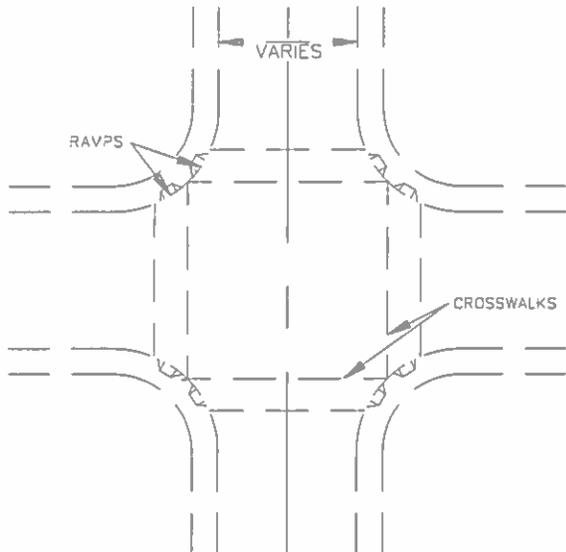


DESIGN-B PARKING PROHIBITED

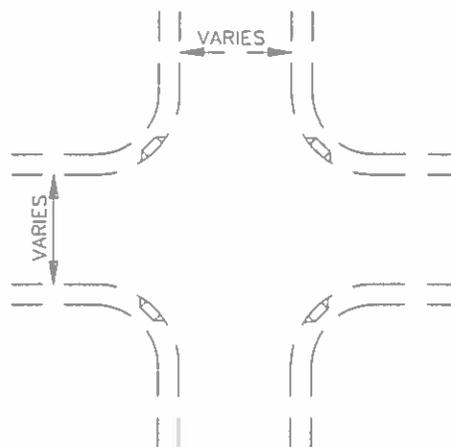
3-120

INTERSECTION PLANS AND CURB RAMP LOCATIONS

- Notes:**
1. Catch basin and inlets shall be outside the curb ramp (24" min. Clearance from ramp).
 2. Care shall be taken to keep the ramp from conflicting with hydrants, poles, inlets, and other utilities.
 3. When ramps are constructed on one side of street, ramps shall be constructed at corresponding locations on opposite side of street.
 4. Minimum 20' R. local and minor collector, 35' R. major collector and arterial.

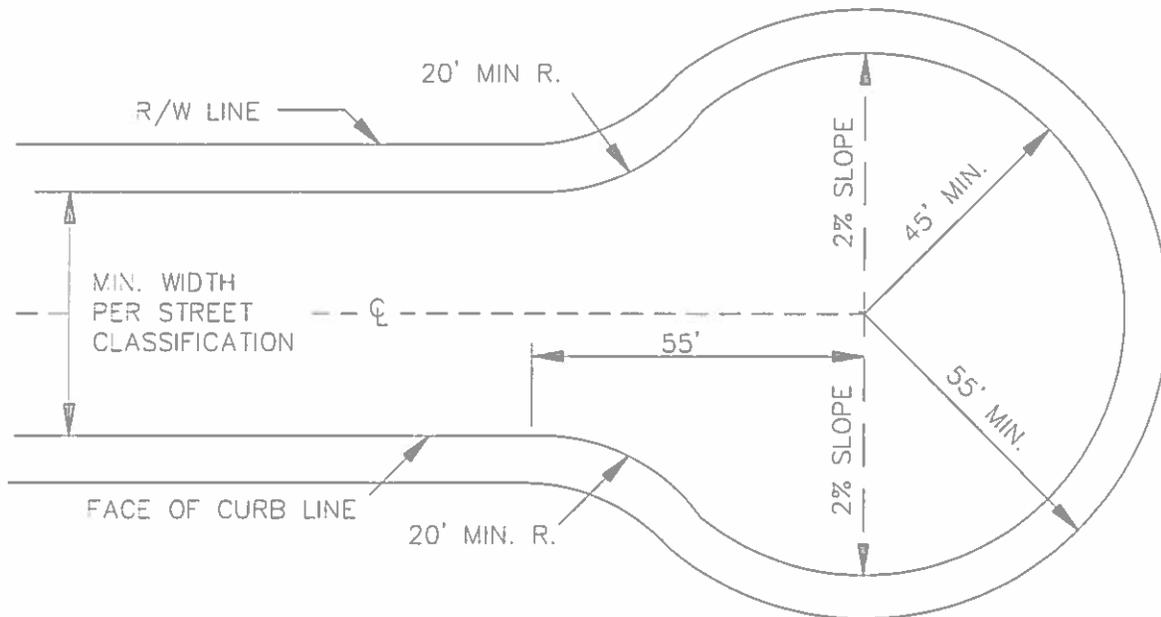


TYPICAL ROAD INTERSECTION



ID-038

CUL-DE-SAC PLAN



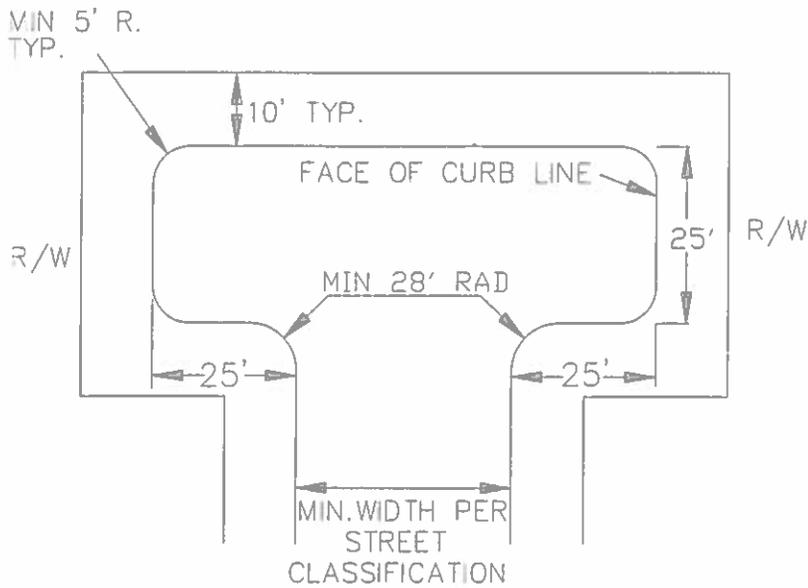
NOTE:

A "Hammerhead" or "L" turnaround may be used, with City Engineer approval, as an alternate when a cul-de-sac is not feasible, or as a means of intermediate turnaround for permanent streets longer than 150'. ID-039

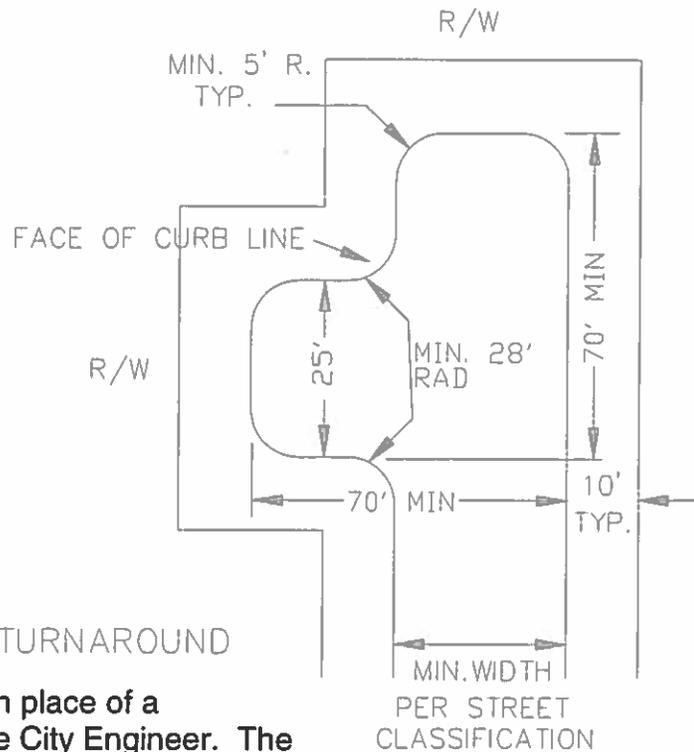
A street that is planned to dead-end against a subdivision boundary for future extension, shall have temporary easement for right-of-way and be paved without curb and gutter for ease of removal of extra pavement and easement when street is extended.

At least one sign stating "No parking - Fire Lane" shall be installed at the beginning of Cul-de-sac by developer.

OTHER TYPE TURNAROUND PLANS



"T" OR HAMMERHEAD TURNAROUND



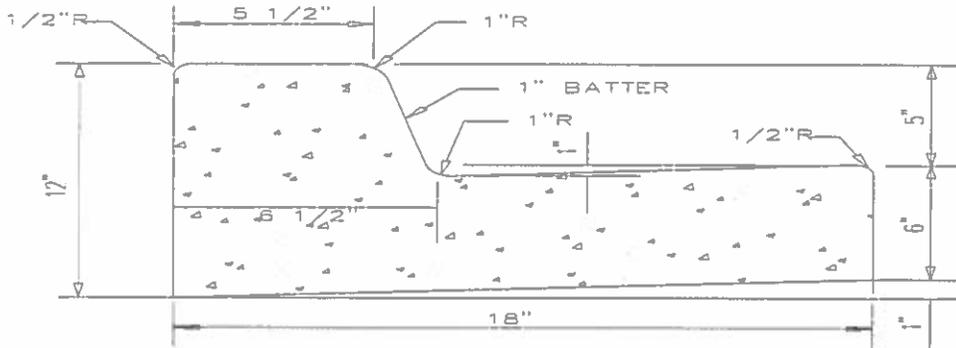
"L" TURNAROUND

NOTE: These plans may be used in place of a cul-de-sac only upon approval of the City Engineer. The turnaround must be clearly marked or signed "No parking - Fire Lane" with as many signs as is necessary at the beginning of the turnaround, installed by the owner/developer.

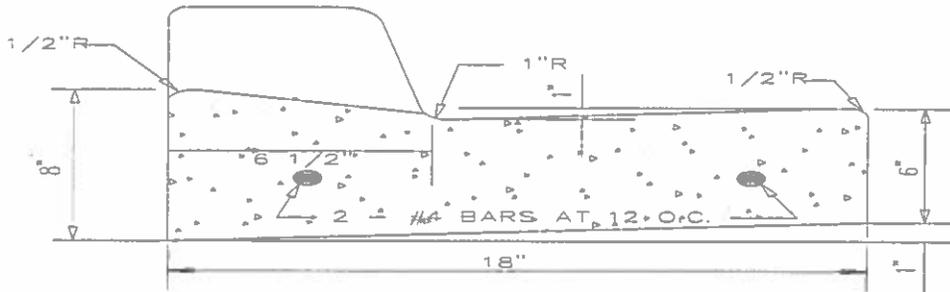
ID-040

CURB & GUTTER SECTIONS

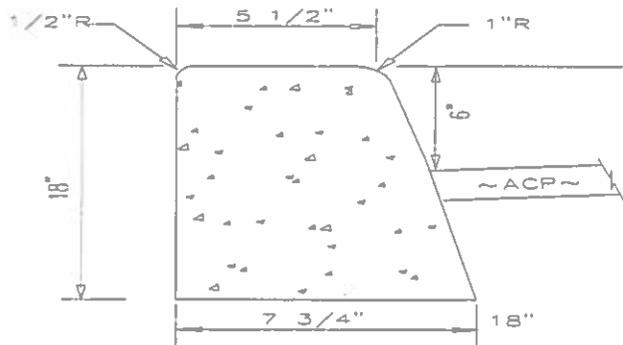
MONOLITHIC POUR W/SIDEWALK PERMITTED W/3/8" EXPANSION JOINT FELT OR CURB & GUTTER SHALL BE POURED SEPARATELY FROM SIDEWALK



CURB & GUTTER SECTION
TYPE "A"

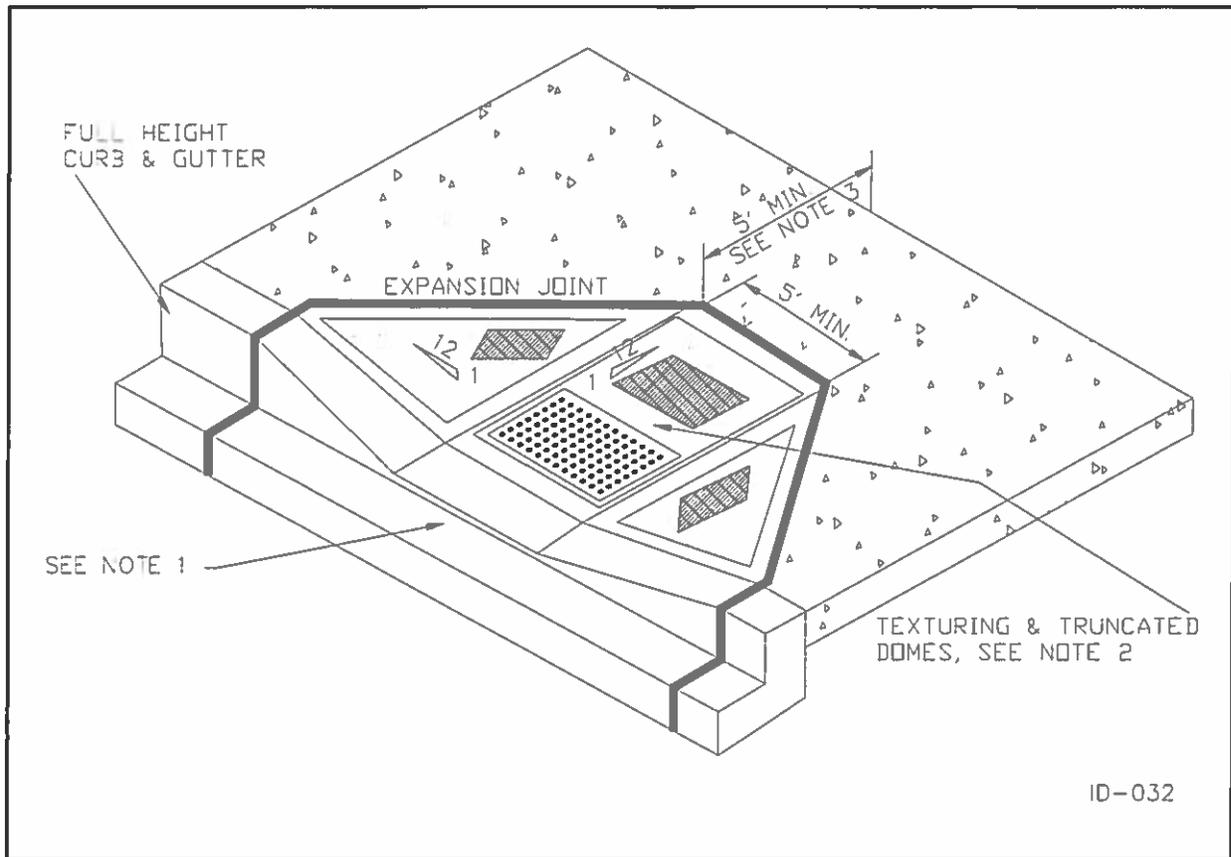


CURB & GUTTER @ DRIVEWAY
TYPE "D"



CURB SECTION
TYPE "E" - SPECIFIC APPROVAL REQUIRED

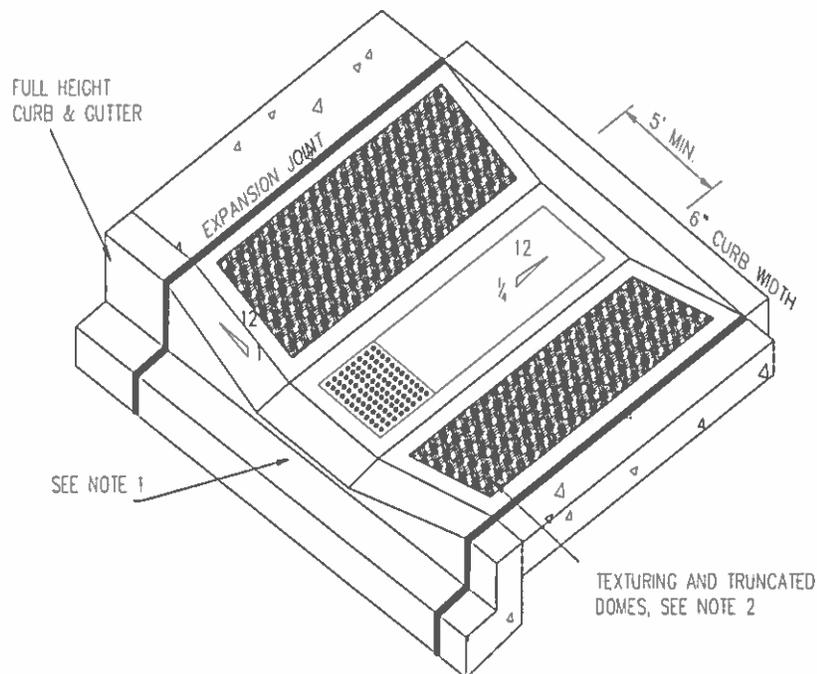
ID-007

WHEELCHAIR RAMP DETAIL WITH LANDING

1. Depressed curb shall have no more than a 1/2" radius lip at gutter in wheelchair ramp sections.
2. Texturing with metal grid placed in wet concrete and then removed to leave ridged surface pattern. Texturing shall cover upper sloped area in ramp and shall not exceed 1/2" depth. Contractor to provide sample for approval by engineer prior to performing work. ADA truncated domes as per WSDOT standards. Dome material as approved by Public Works.
3. Minimum 5' landing, or use detail #6-15 without landing.
4. See also WSDOT Standard Plan F-3a.

WHEELCHAIR RAMP DETAIL WITHOUT LANDING

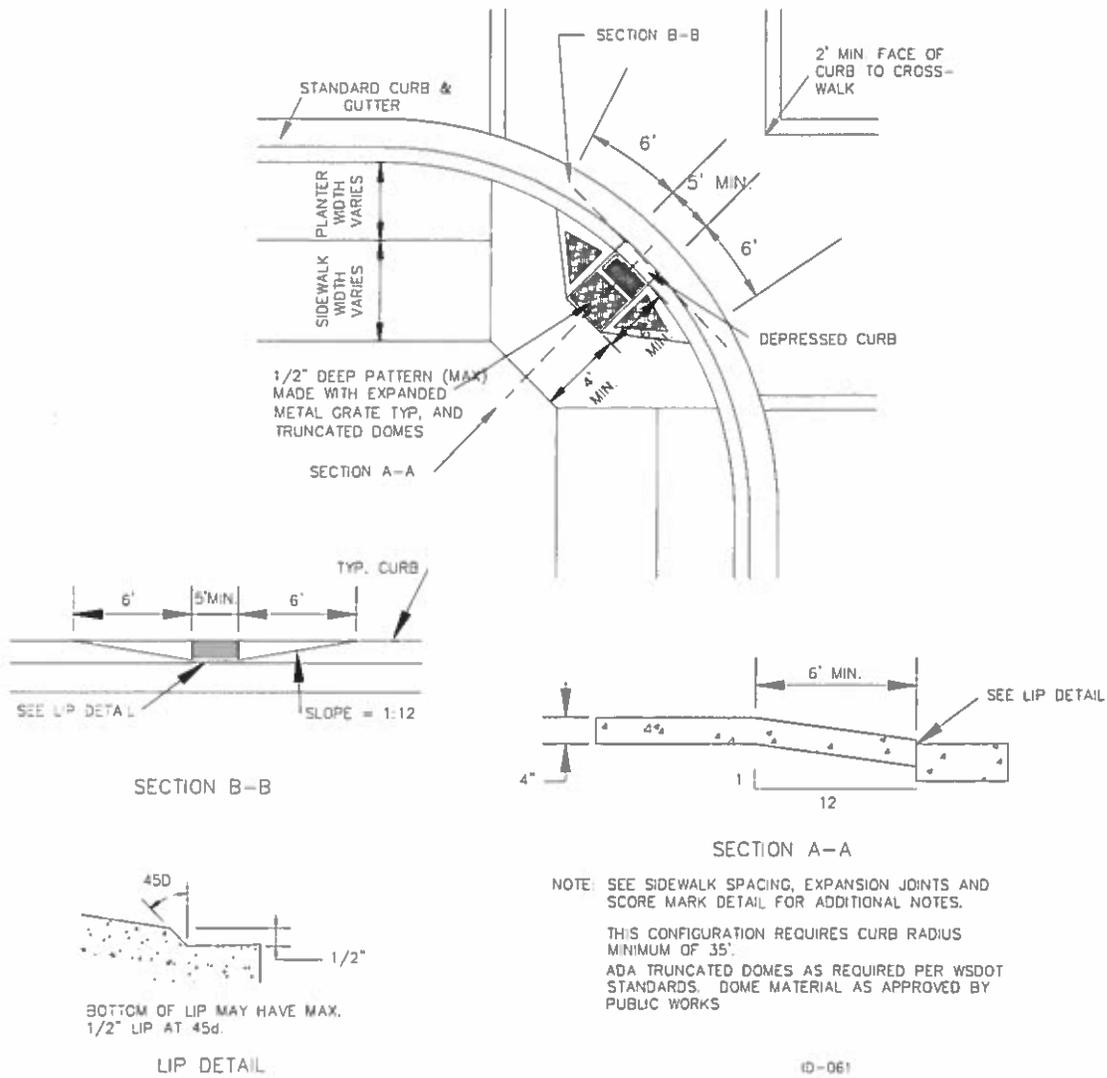
1. Depressed curb shall have no more than a $\frac{1}{2}$ " radius lip at gutter in wheelchair ramp sections.
2. Texturing with metal grid placed in wet concrete and then removed to leave ridged surface pattern. Texturing shall cover all sloped area in ramp and shall not exceed $\frac{1}{2}$ " depth. Contractor to provide sample for approval by engineer prior to performing work. ADA truncated domes as per WSDOT Standards . Dome material as approved by Public Works.
3. See also WSDOT Standard Plan F-3b.



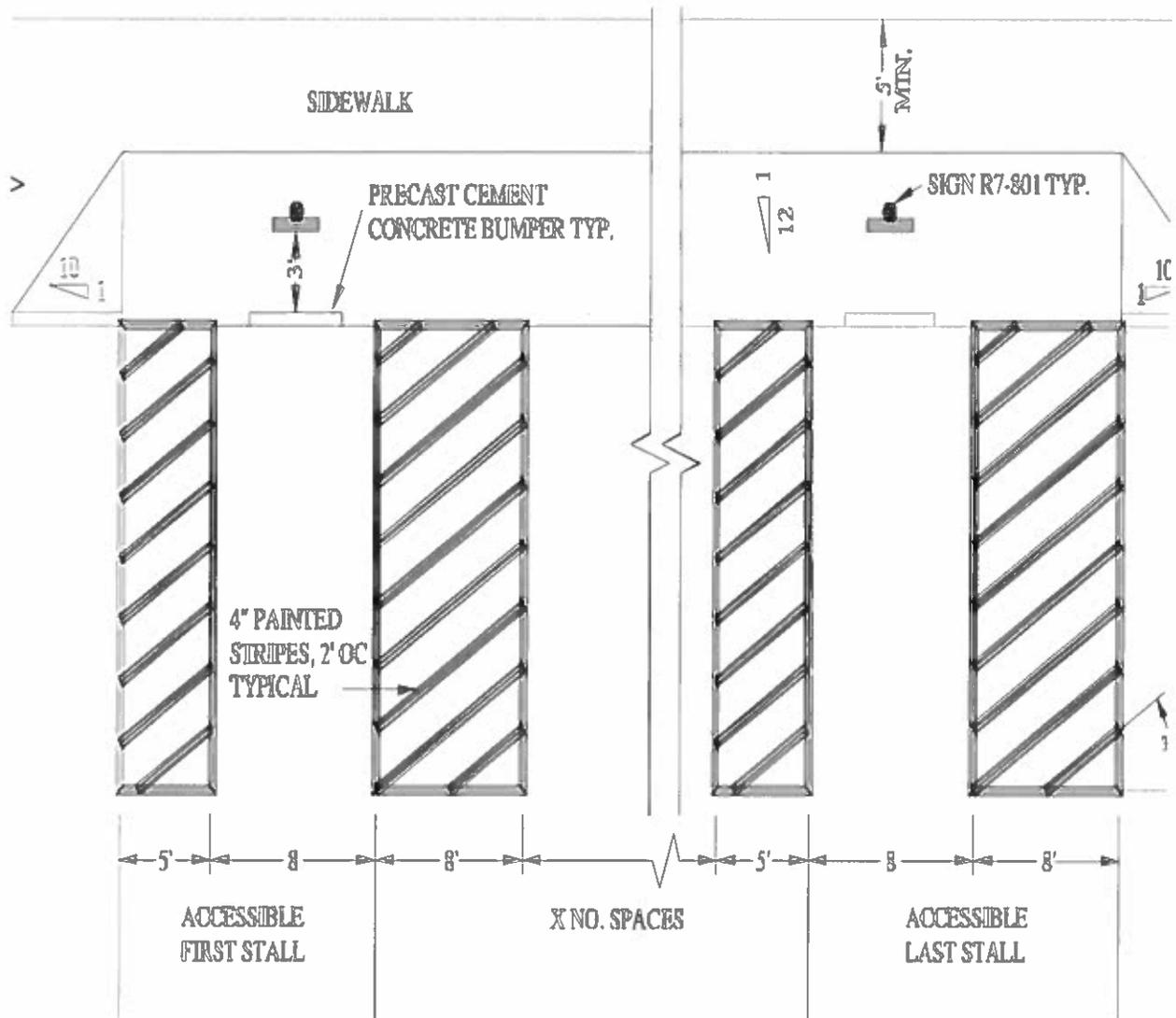
WHEELCHAIR RAMP WITHOUT LANDING

ID-041

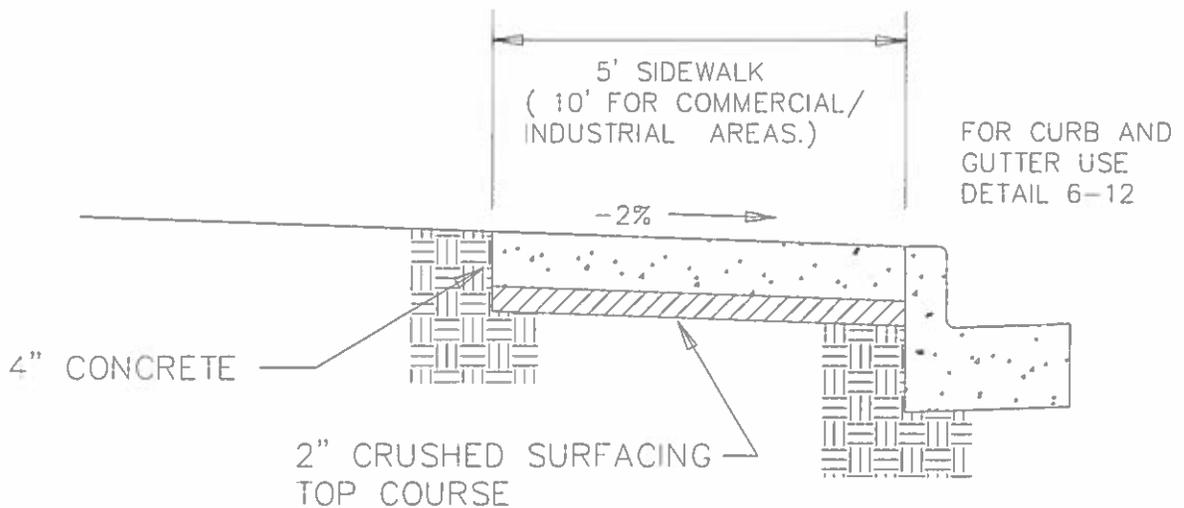
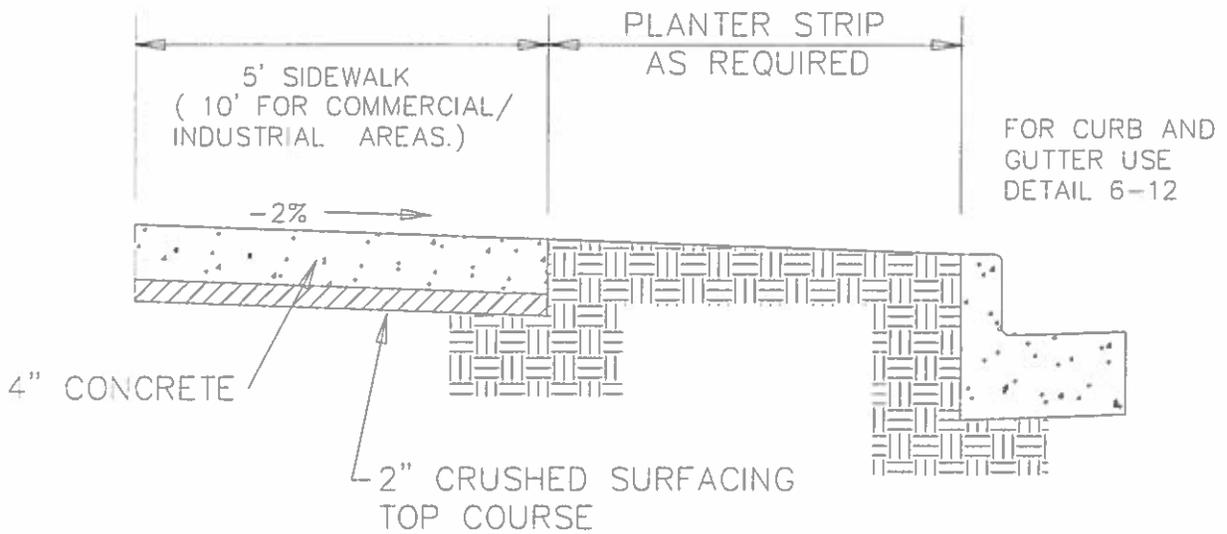
HANDICAP RAMP TYPE I



ACCESSIBLE PARKING SHOWN WITH CONTINUOUS CURB RAMP



SIDEWALK

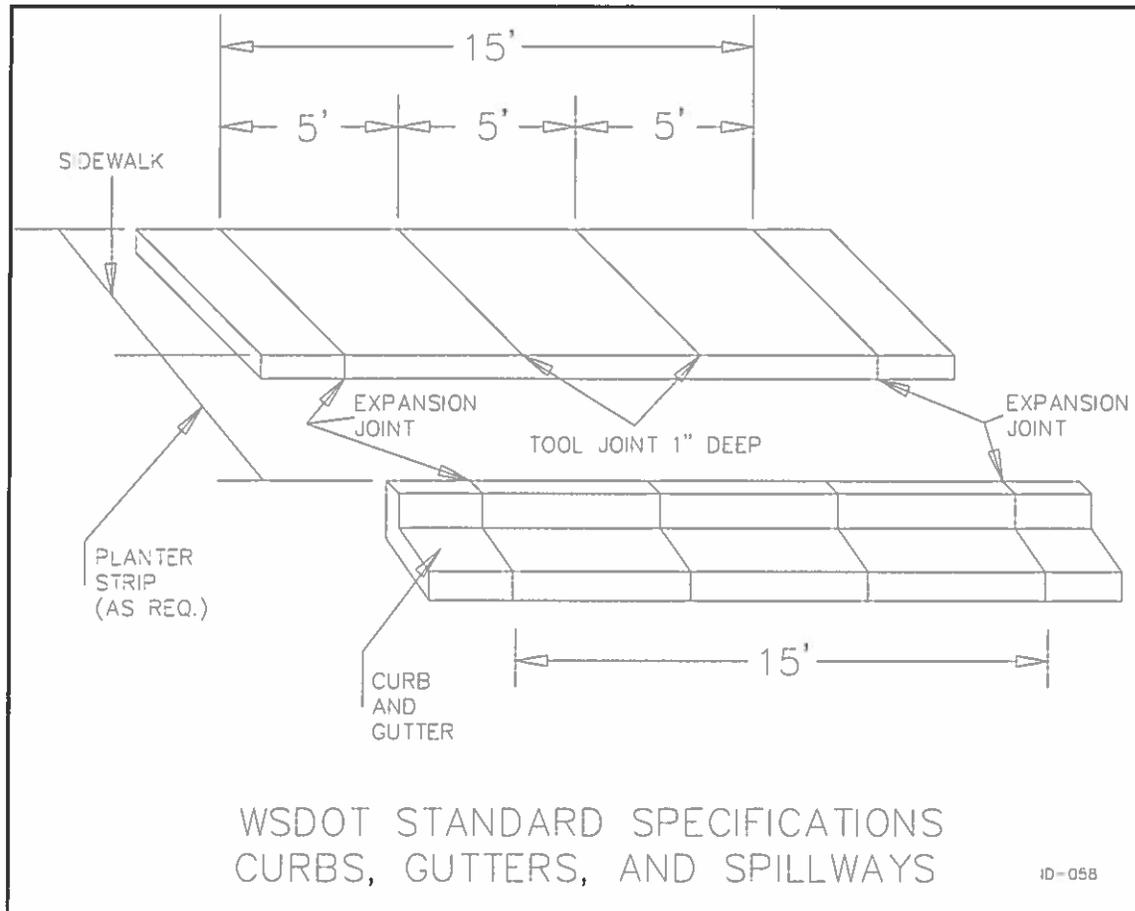


ID-059

GENERAL NOTES:

1. For joints and scoring, see standard for sidewalk spacing, expansion joints, and score marks.
2. Concrete driveways require a minimum depth of 6".

SIDEWALK SPACING



GENERAL NOTES:

1. Expansion joint material to be 3/8" thick premolded joint filler full thickness of concrete.
2. Form and subgrade inspection required before pouring concrete.
3. Score marks shall be $\pm 1/8$ " wide by ± 1 " deep for sidewalks 10' in width, a longitudinal score mark shall be made along center of walk.
4. Expansion joints shall be installed in curb and gutter and in sidewalk at PC and PT at all curb returns. Expansion joints shall be placed in sidewalk at same locations as those in curb and gutter when sidewalk is adjacent to curb and gutter, unless otherwise directed by Public Works.
5. Brush finish.
6. Extruded cement concrete curb allowed with approval of City Engineer.

ACCESS APPROACH CONTROL (DRIVEWAYS & PRIVATE ROADS)

A residential driveway is an access which serves one, two, or three single family residences. Recorded easements are necessary for such joint use access. All other accesses are commercial access.

Width shall be determined as the net width of opening, exclusive of side slopes.

The maximum width of residential driveway curb cuts shall be 25', minimum 10'. If a parcel is entitled to two accesses, these may be combined into one single access of 35' maximum width. (Does not apply to rolled curb section.)

The maximum width of commercial access shall be 35', minimum 15'.

Not more than one access shall be permitted for frontage 75' or less. Two accesses shall be permitted for frontage greater than 75'. Permission to have three or more accesses on a frontage greater than 150' must be approved by the engineer.

As a condition of issuance of any Right of Way Permit, all abandoned access improvements on the same frontage shall be removed and the curbing and sidewalks properly restored. Location and extent of all commercial access is subject to approval of the engineer.

No concrete shall be poured until forms are inspected by engineer.

Joint use accesses are encouraged in commercial areas.

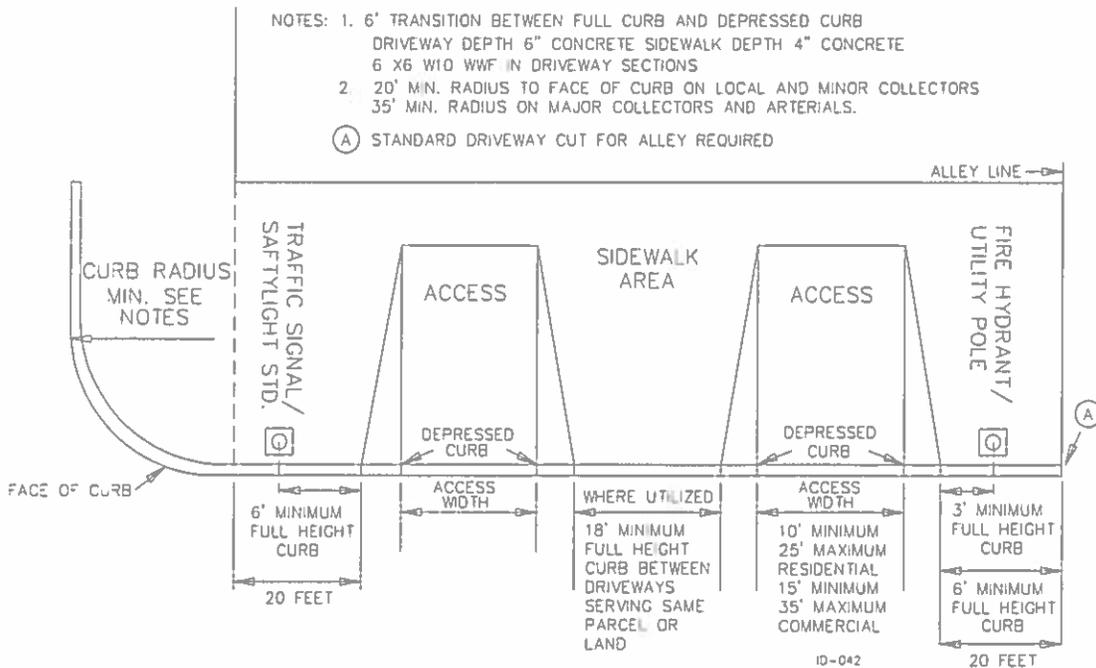
Upon remodel of existing commercial accesses, standards may be modified to allow minimum turning radius required for related commerce as approved by the City engineer.

The centerline of the approach extended, shall intersect the centerline of the City street at an angle not less than 75°.

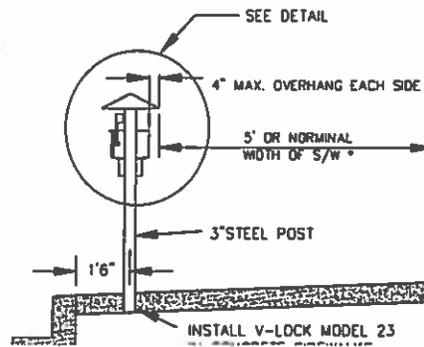
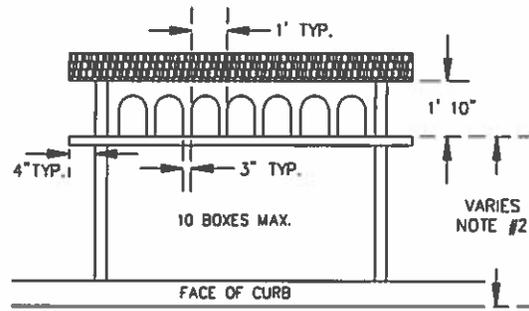
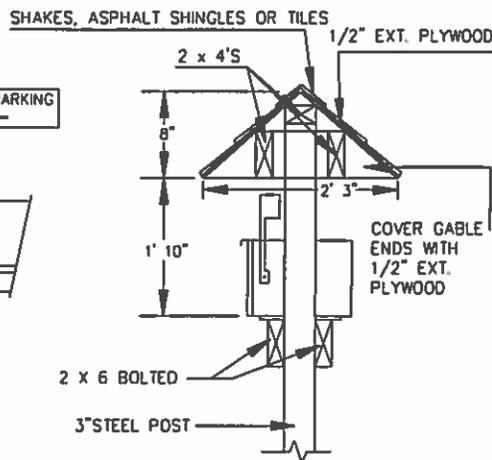
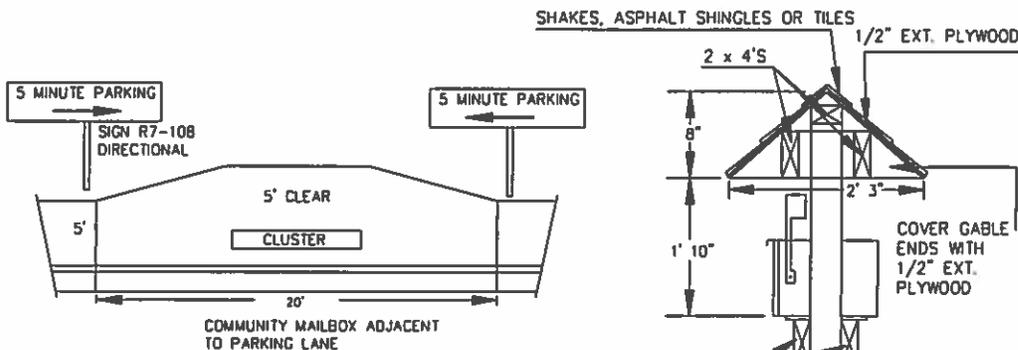
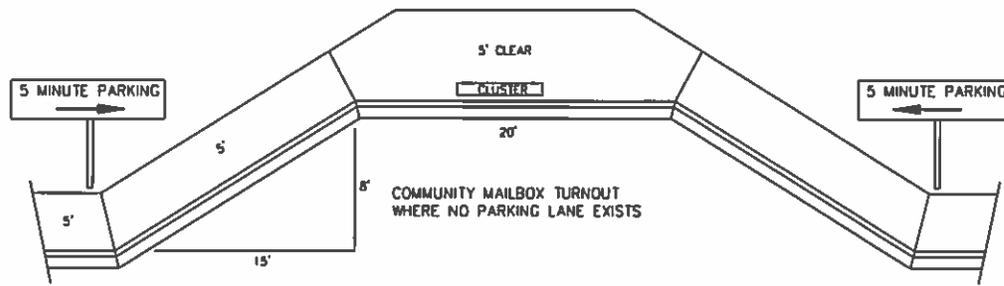
Wherever topographic conditions permit, there shall be a level section on the approach extending back from the road shoulder a distance of fifteen feet (15').

Road approaches must be ballasted and leveled with crushed stone surfacing (standard top course.) Approaches shall be paved and must slope away from the road shoulder at a minimum rate of three inches (3") per ten feet (10'). Maximum driveway slope shall not exceed 18%.

No access approach shall be constructed until a permit from the City has been issued.

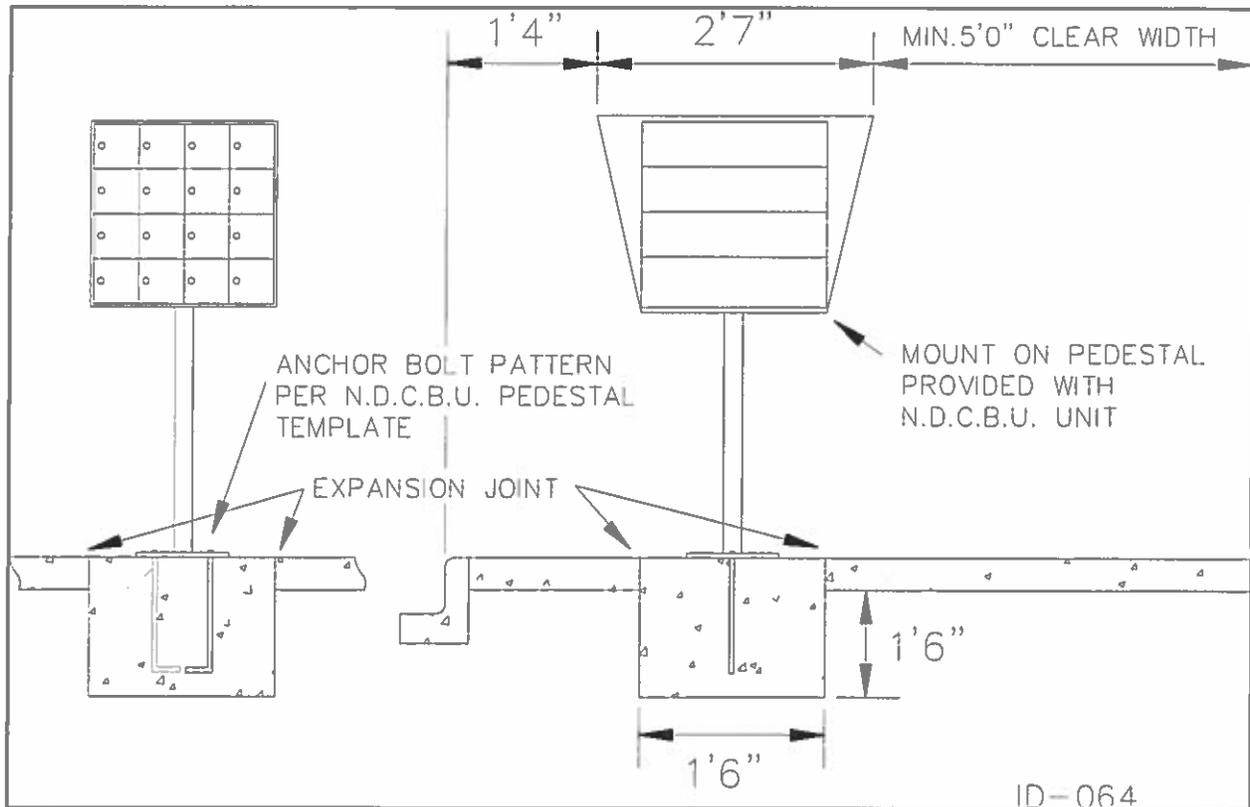


MAILBOX MOUNTING CURB TYPE INSTALLATION



NOTE: SEE SHEET 6 - 22 FOR GENERAL NOTES AND ALTERNATE LOCATION SCHEME FOR MANUFACTURED DELIVERY / COLLECTION BOX UNITS

NEIGHBORHOOD DELIVERY AND COLLECTION BOX UNIT

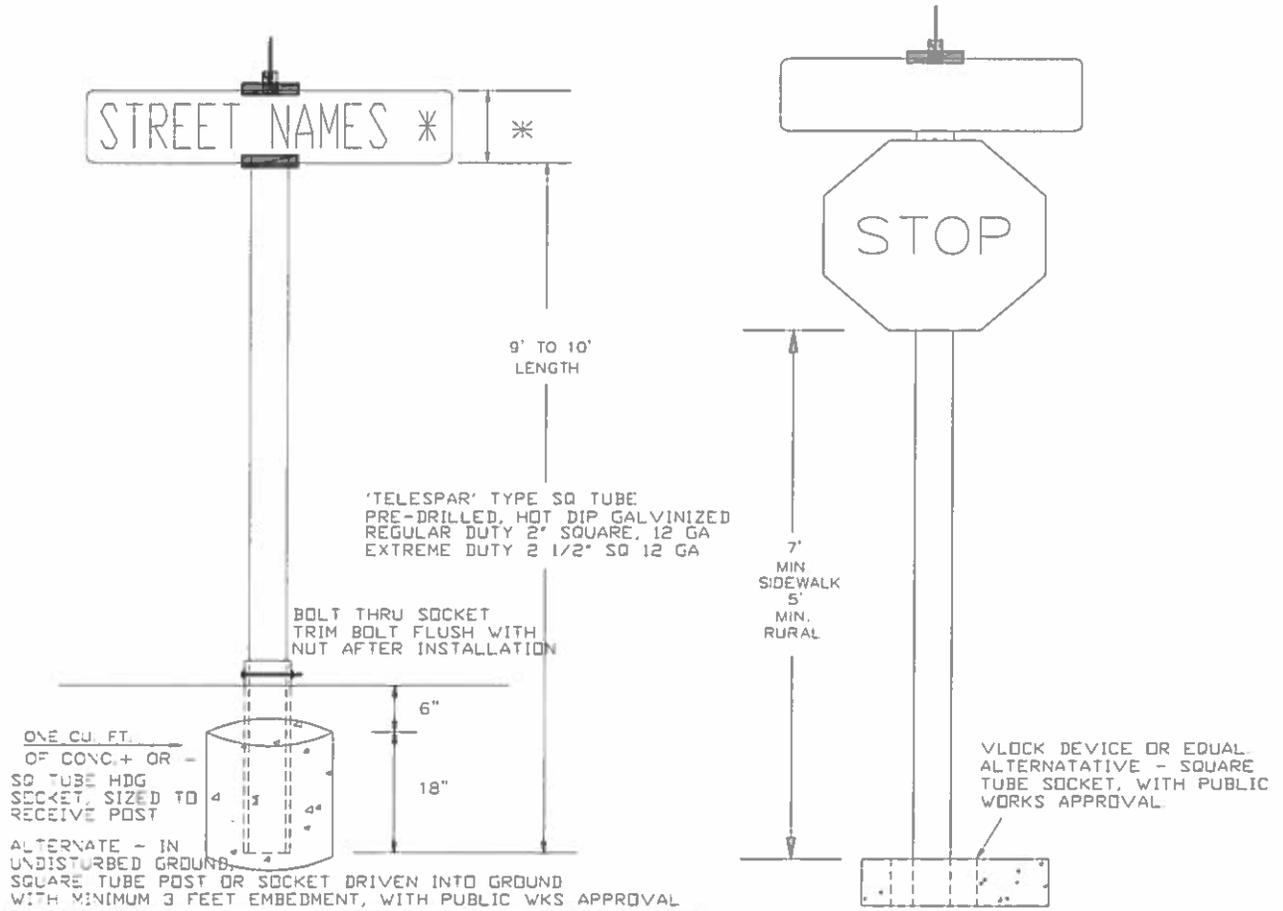


NOTE: SEE SHEET 6 - 1 FOR TURNOUT DETAILS

Mailbox Locations Shall Be:

1. For three or more mailboxes on urban residential access streets where the speed limit is 25 mph.
2. Mailbox height varies according to the type of delivery vehicle. These heights shall be determined by the postmaster during plan review.
3. Mailboxes must be postmaster approved with a uniform box style and method of address identification.
4. Location of mailboxes subject to approval of postmaster and City engineer.
5. Optional newspaper box structures are subject to approval of postmaster and City engineer.
6. Roof is optional.
7. The minimum spacing between the centers of support posts shall be three-fourths the height of the posts above the groundline.
8. Mailbox support designs not described in this regulation will be acceptable if approved by the chief engineer of the United States Post Office.
9. It will be the responsibility of the postal patron to inform the USPO of any new or existing mailbox installation where shoulder construction is inadequate to permit all-weather vehicular access to the mailbox.
10. Any mailbox that is found to violate the intent of this regulation shall be removed by the postal patron upon notification by the USPO. At the discretion of the USPO, based on an assessment of hazard to the public, the patron will be granted not less than 24 hours nor more than 30 days to remove an unacceptable mailbox. After the specified removal period has expired, the unacceptable mailbox will be removed by the USPO at the postal patron's expense.

SIGN INSTALLATION



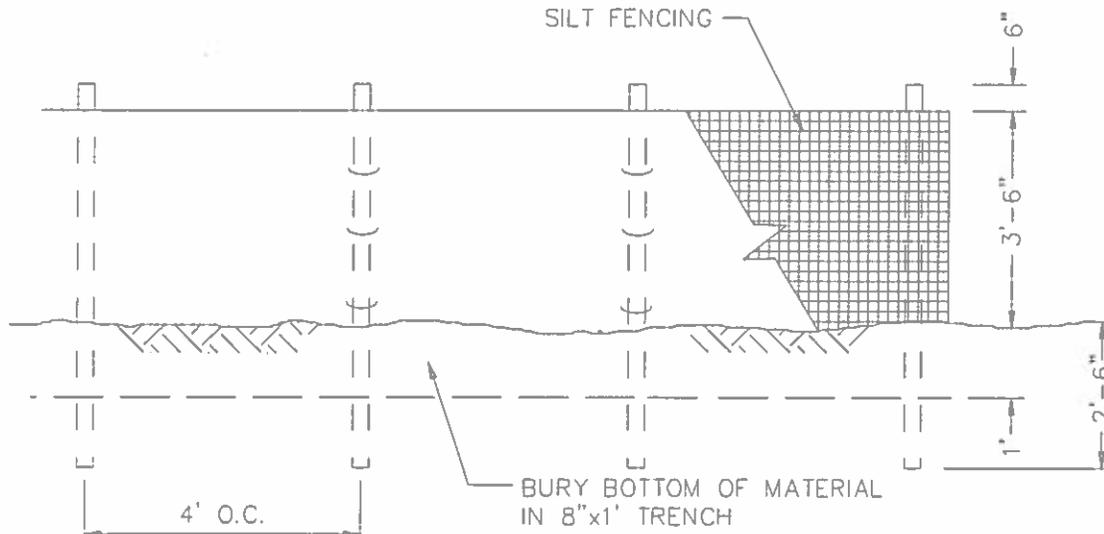
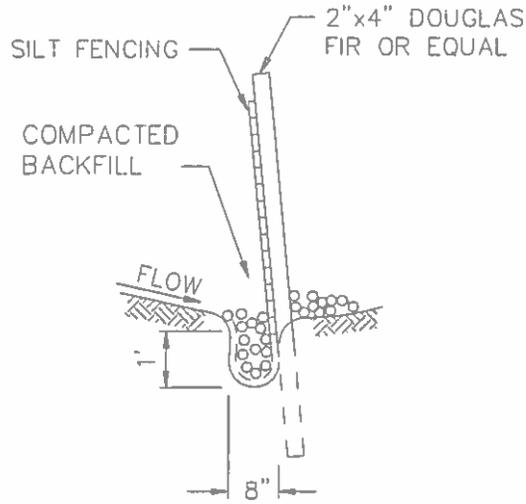
SIDEWALK INSTALLATION

ALL SIGN TYPES AND INSTALLATIONS ARE GOVERNED BY CURRENT MUTCD.
ALL SIGNAGE REFLECTIVITY SHALL BE HIGH INTENSITY GRADE AT MINIMUM

- * STREET NAME SIGNS ARE ALL ALUMINUM, 0080" MATERIAL WITH LEGEND COLOR WHITE AND BACKGROUND GREEN, AND NAMES ON BOTH SIDES. LETTERS ARE STANDARD 'B' SERIES, PREFIX AND SUFFIX ABBREVIATIONS ARE STANDARD 'C' SERIES. SPEED ZONES LESS THAN 30 MPH MAY HAVE 6" SIGNS WITH 4" LETTERS. SPEED ZONES AT OR GREATER THAN 30 SHALL HAVE 9" SIGNS WITH 6" LETTERS

10-029

SILT FENCING

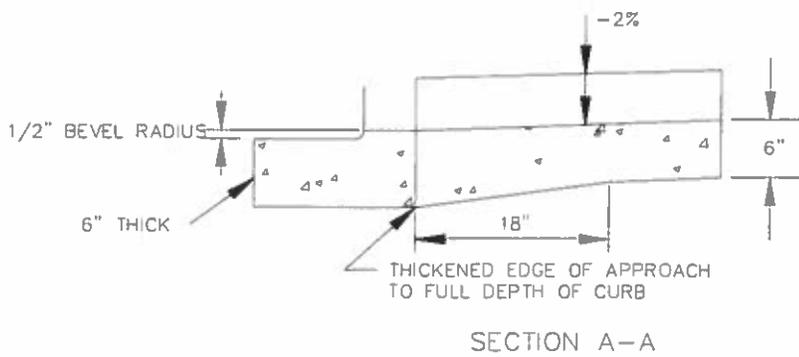
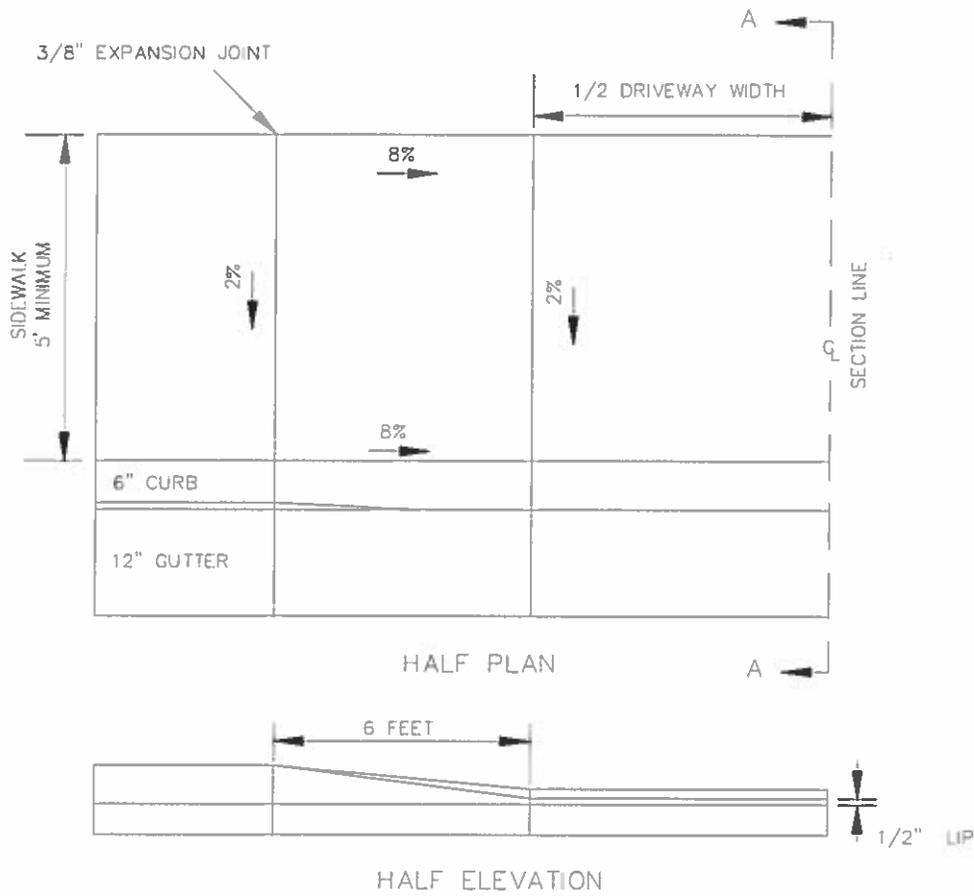


SILT FENCE DETAIL

N.T.S.

ID-031

DRIVEWAY WITH DEPRESSED SIDEWALK

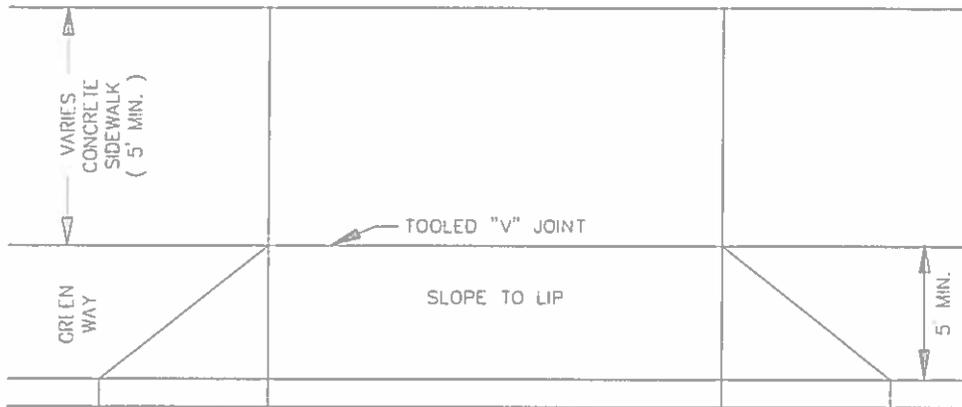


NOTES:

- 1 CEMENT CONCRETE APPROACHES SHALL BE CONSTRUCTED OF AIR-ENTRAINED CONCRETE CLASS B
- 2 CONTRACTION JOINTS SHALL BE PLACED AT 15' MAXIMUM SPACING.
- 3 ALL JOINTS SHALL BE CLEANED AND EDGED.

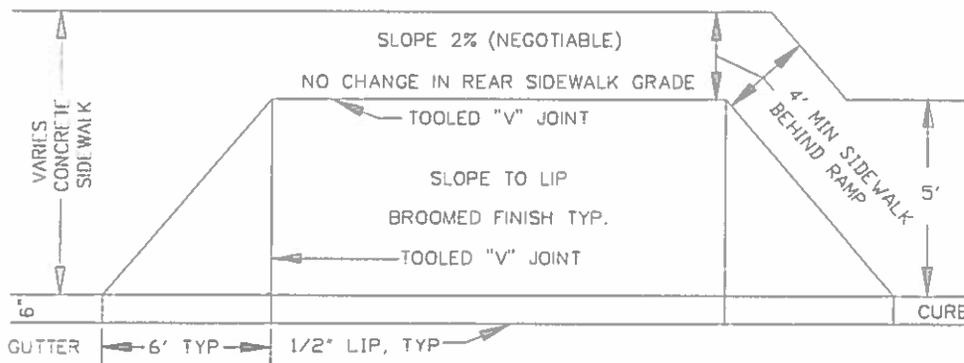
ID-043

TYPICAL DRIVEWAY WITH CURB/WALK, WITH AND WITHOUT GREEN STRIP

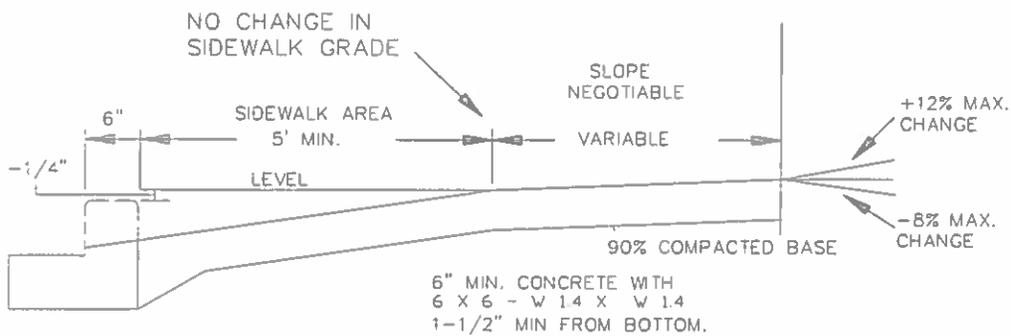


TYPICAL DRIVEWAY W/GREENWAY

6" MIN. 3,300 PSI REINF. CONC



TYP DRIVEWAY WITH SIDEWALK 5' OR GREATER



SEE ALSO WSDOT STANDARD PLAN F-4

ID-116